



# PORT LANDS PLANNING FRAMEWORK

## Purpose / Elements of the Planning Framework

### The Purpose of the Port Lands Planning Framework is to:

- Integrate the other planning initiatives currently underway
- Update and refresh the vision for the Port Lands
- Provide a comprehensive picture of how the area should redevelop over the long-term and reconcile competing interests
- Provide a flexible/adaptable planning regime
- Ensure sustainable community building
- Ensure that public and private investments contribute to the long-term vision and have lasting value
- Provide the basis for Official Plan amendments
- Resolve Ontario Municipal Board appeals of the Central Waterfront Secondary Plan

### Elements of the Planning Framework:

- An overall vision for the Port Lands and development objectives
- A connections plan which will identify:
  - Major and intermediate streets
  - Major pedestrian and cycling facilities
  - A transit plan that also addresses City Council direction
- Generalized land use direction
- Identification of character areas
- A parks and open space plan which will define:
  - Green corridors
  - District / Regional parks
  - Water's Edge Promenades
- A heritage inventory and direction for listing/designating heritage resources
- Urban design principles and structure plan:
  - Built form and building typologies
  - Special sites (catalyst uses)
  - Relationship of development to major public spaces
  - Urban design context for heritage features
  - Identification of major views
- A high -level community services and facilities strategy
- Implementation and phasing direction

## PROCESS



# PORT LANDS PLANNING FRAMEWORK

## Port Lands Acceleration Initiative Plan (PLAI)

### Port Lands Acceleration Initiative (PLAI)

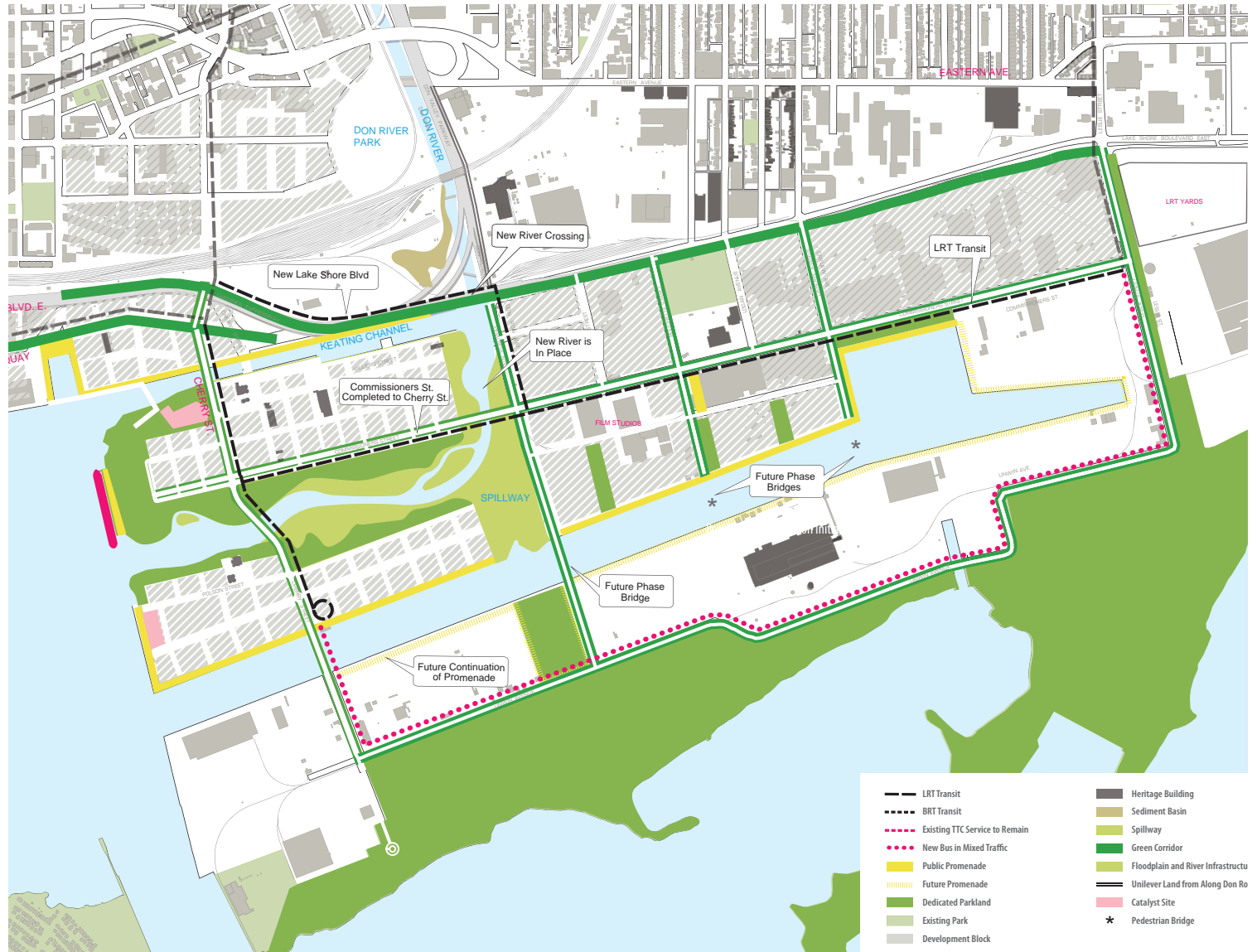
The PLAI was initiated in October 2011 to:

- Refine the Don Mouth Naturalization and Port Lands Flood Protection (DMNP) EA; and
- Develop a business and implementation plan with the objective of accelerating development opportunities in the Port Lands.

The PLAI was endorsed by City Council in October 2012.

It provides direction for moving forward with the revitalization of the Port Lands. It envisions developing new mixed-use urban districts built out over the long-term and centred on the following:

- Phasing of development;
- Phasing of transit;
- Identification of catalyst uses and sites lending distinction to the Port Lands;
- Green corridors along Cherry Street, Don Roadway, Bouchette Street, Carlaw Avenue and Leslie Street which would be spines linking the Port Lands with the rest of the city;
- The development of spectacular waterfront parks and plazas; and
- Maintaining industrial and port uses.



# PORT LANDS PLANNING FRAMEWORK

## DMNP EA & Lower Don Lands Class EA

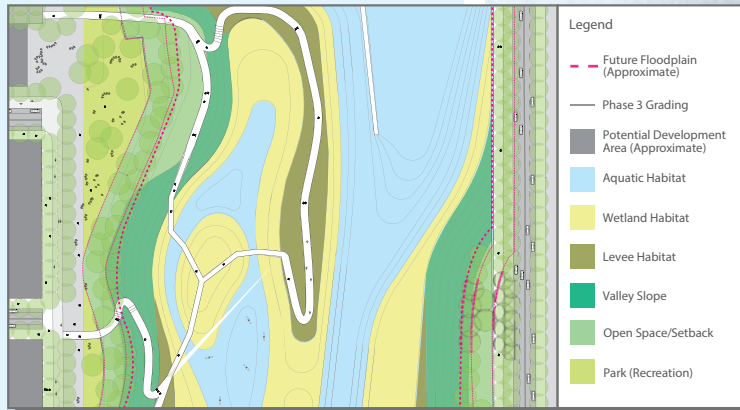
### The Don Mouth Naturalization and Port Lands Flood Protection (DMNP) EA establishes:

- Keating Channel, new river flood plain, and Greenway for flood conveyance;
- Naturalization within the river valley while also enabling city building;
- An adaptive management strategy;
- Proposed phasing for the construction of the river valley and the necessary flood protection requirements; and
- Minimum elevations for surrounding lands.

### Lower Don Lands Class EA establishes:

- Infrastructure (streets, transit, water, sanitary and stormwater) necessary to support revitalization and to coincide with the river valley; and
- Minimum elevations of bridges and streets to match the DMNP

### Preferred DMNP and Lower Don Lands Plan (2013)



Conceptual plan for the Don Greenway north of Commissioners Street

# PORT LANDS PLANNING FRAMEWORK

## CENTRAL WATERFRONT SECONDARY PLAN

The **Central Waterfront Secondary Plan (CWSP)** was adopted by City Council in 2003.

It was amended in 2010, through Official Plan Amendment (OPA) 388, to implement the outcomes of the Lower Don Lands Framework Plan.

The majority of the CWSP remains under appeal at the Ontario Municipal Board and is not in effect.

The Central Waterfront Secondary Plan requires updating to reflect the objectives and outcomes of the **Port Lands Acceleration Initiative (PLAI)** adopted by City Council in 2012, providing opportunities to:

- Introduce more robust policies / direction to guide the revitalization of the Port Lands, and
- Resolve existing Ontario Municipal Board appeals of the CWSP

### Port Lands Acceleration Initiative Plan (2012)



Central Waterfront Secondary Plan: Map B Land Use Plan (as amended in 2010 by OPA 388)



The CWSP is centered on **four core principles**:

- Removing Barriers and Making Connections
- Building a Network of Spectacular Waterfront Parks and Public Spaces
- Promoting a Clean and Green Environment
- Creating Dynamic and Diverse New Communities

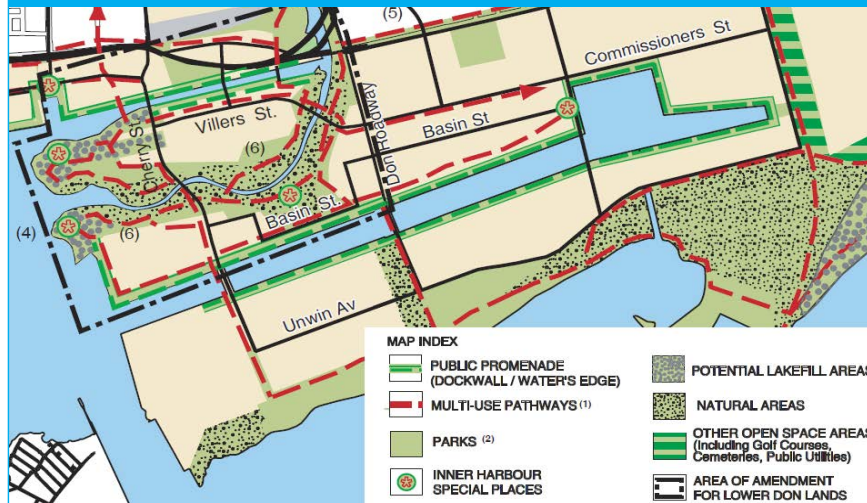
The CWSP designates the majority of the Port Lands **Regeneration Areas** and envisions a range of different activities occurring in the Port Lands, including:

- The creation of new, urban districts and neighbourhoods set amid Toronto's ongoing port activities
- New media, knowledge-based businesses and "green" industries
- Downtown-serving and marine-related industries
- Recreational, cultural and tourist amenities

Additional **policy direction** in the CWSP for the Port Lands, includes, but is not limited to:

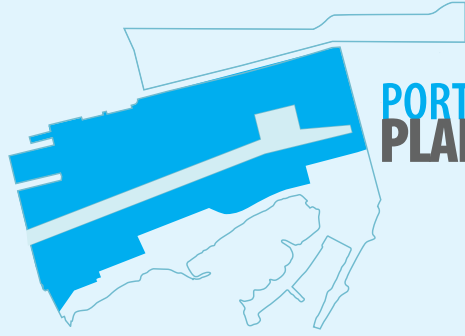
- Renaturalizing the Don Mouth and creating the Don Greenway
- Incorporating the Ship channel as a focal point and unique amenity
- Creating complete communities generally developed at a medium scale with some taller buildings at appropriate locations with necessary schools and community facilities
- Reinforcing visual connections and improving physical connections between the city and waterfront
- Transforming Lake Shore Boulevard into an urban avenue with pedestrian and cycling facilities
- Showcasing the City's commitment to the development of a clean and green waterfront

Central Waterfront Secondary Plan: Map C Parks and Open Space Plan (as amended in 2010 by OPA 388)



# PORT LANDS PLANNING FRAMEWORK

## EXISTING CONDITIONS



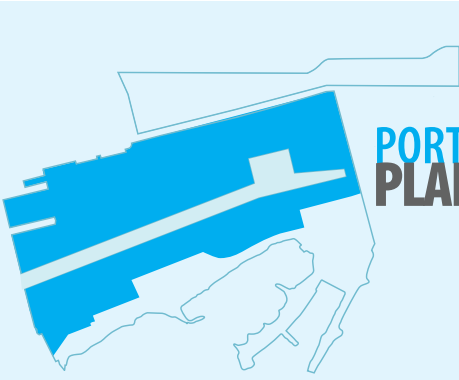
### Port Lands Scale



Comparison of the Scale of the Port Lands



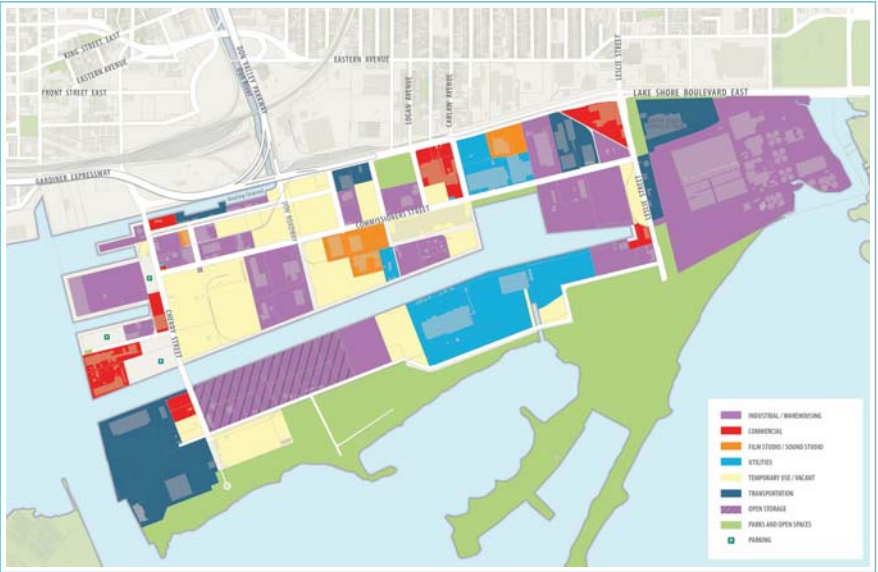
Neighbourhoods/Districts Overlay



# PORT LANDS PLANNING FRAMEWORK

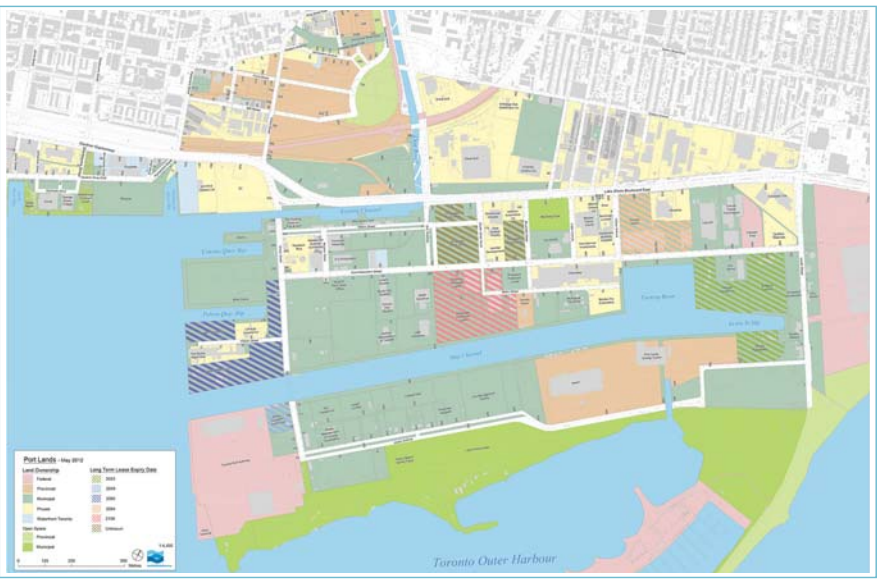
## Existing Conditions

### Existing Land Uses



Municipal Yards

### Land Ownership

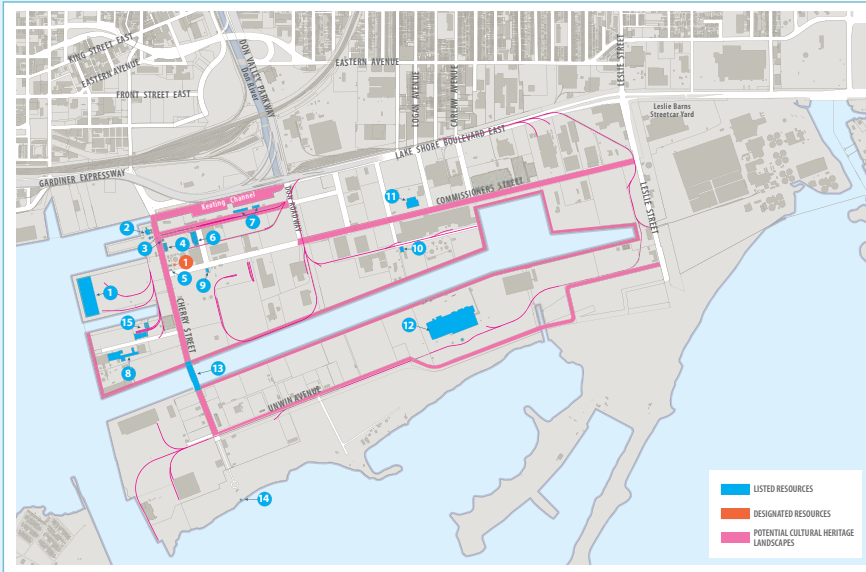


# PORT LANDS PLANNING FRAMEWORK

## Existing Conditions

### Views and Vistas

### Heritage Resources



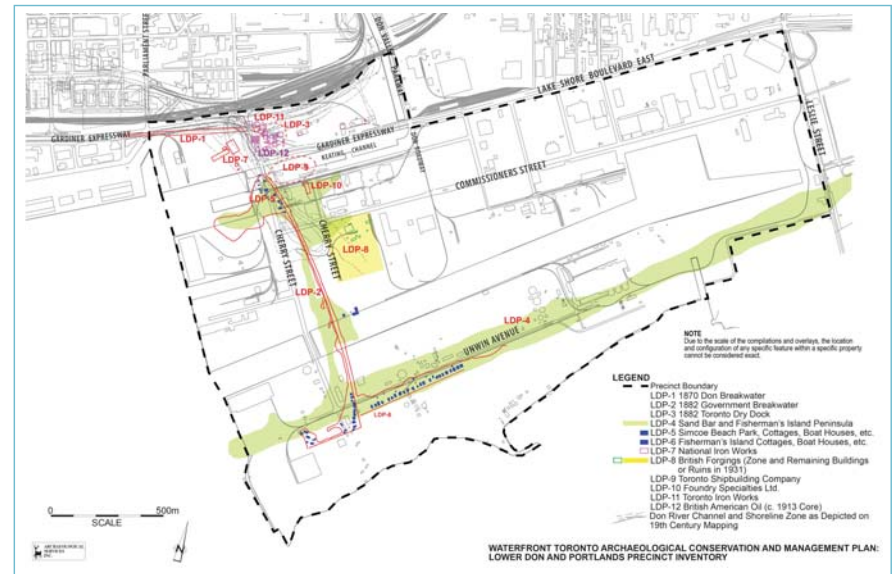
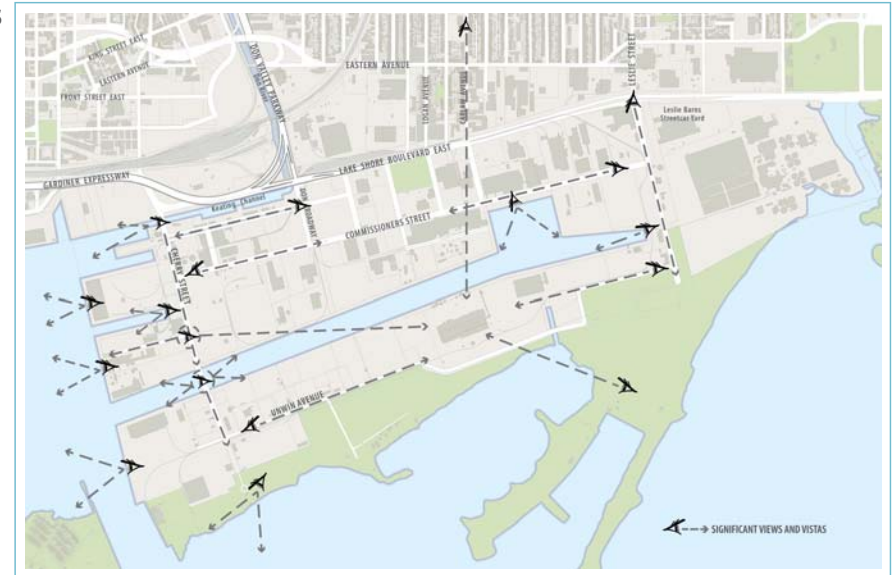
#### Listed Buildings and Structures

- 1 Marine Terminal 35 and Atlas Crane
- 2 Century Coal Company Building
- 3 Bank of Montreal Building
- 4 William McGill and Company Building
- 5 Dominion Bank Branch
- 6 Queen's City Foundry
- 7 Toronto Harbour Commissioners Storage Buildings
- 8 Dominion Boxboards Building
- 9 Fire Hall No. 30
- 10 Sun Oil Company Building
- 11 City of Toronto Incinerator Building
- 12 Hearn Generating Station
- 13 Cherry Street Bascule Bridge
- 14 Cherry Beach Life Saving Station
- 15 Lafarge

#### Designated Buildings and Structures

- 1 Toronto Hydro Substation Building

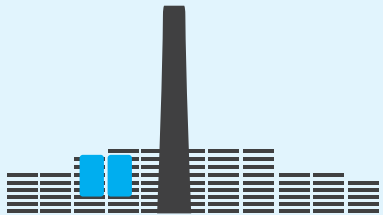
### Archaeological Resources







1 HEARN BUILDING



**Hearn Generating Plant**

Existing Floor Area: 33,000m<sup>2</sup>  
 Building Footprint: 24,000m<sup>2</sup>  
 Building Height: 41.5 metres  
 Chimneystack Height: 213 metres  
 Land Use: Vacant

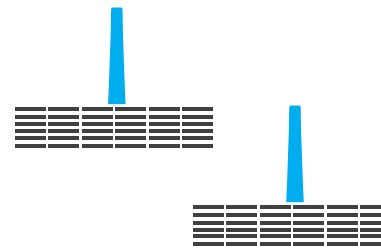
3 MAPLE LEAF GARDENS  
= 1 Hearn Building



**Maple Leaf Gardens (Toronto, Canada)**

Reuse of the former NHL hockey venue  
 Floor Area Prior to Redevelopment: 9,100 m<sup>2</sup>  
 Building Footprint: 9,100 m<sup>2</sup>  
 Building Height: 26 metres to flat roof & 46 metres to the dome  
 Floor Area Post Redevelopment: 32,500 m<sup>2</sup>  
 Land Use: Grocery Store, Office and an NHL Sized Hockey Rink

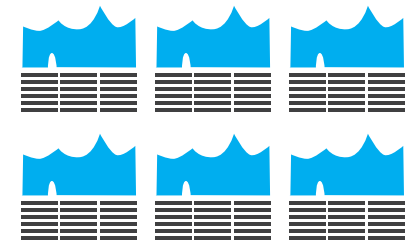
2 TATE MODERNS  
= 1 Hearn Building



**Tate Modern (London, England)**

Reuse of a former power plant  
 Building Footprint: 13,000m<sup>2</sup>  
 Building Height: 35 metres  
 Chimneystack Height: 99 metres  
 Floor Area Port Redevelopment: 34,500 m<sup>2</sup>  
 Land Use: Art gallery

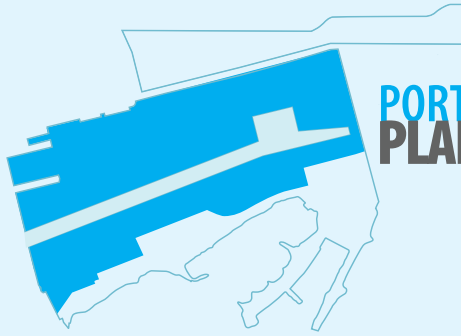
6 ELBPHILHARMONIE HALLS  
= 1 Hearn Building



**Elbphilharmonie Concert Hall (HafenCity, Hamburg)**

Reuse and addition to a former industrial warehouse  
 Building Footprint: 5,000m<sup>2</sup>  
 Building Height with Addition: 110 metres  
 Floor Area Port Redevelopment: 62,000m<sup>2</sup>  
 Land Use: Two concert halls, five-star hotel, 45 residential units, international conference centre, spa and a nightclub

# PORT LANDS PLANNING FRAMEWORK



Development Objectives have been drafted for the Port Lands. These objectives will be used to:

- 1) Evaluate options and alternatives developed in Phase 2 of the study; and
- 2) Form the basis for updating and refreshing the vision for the Port Lands

Do the draft Development Objectives reflect how you see the Port Lands developing?

Provide us with your ideas and suggestions on how to improve these Development Objectives.



## Draft Development Objectives

### CREATING AN INTERESTING AND DYNAMIC URBAN MIX

The revitalized Port Lands is a *dynamic and vibrant area* of the city. A number of new inclusive, sustainable, urban-scaled, compact, mixed-use communities and employment areas are created. Each new urban area will have a *unique local identity and character*. *Water permeates and influences all facets* of the revitalized Port Lands given its proximity to the waterfront, new river valley and continued port activity. A number of *new destinations and special places* are developed which promote walking and taking transit, provide opportunities for social interaction and contribute to *an interesting urban life*.

### CONNECTING THE PORT LANDS TO THE CITY

Enhanced physical, social and visual connections created in the Port Lands *connect the Port Lands to the city*. These connections include a network of fine-grained public streets with animated streetscapes, higher-order transit, new bridges, enhanced pedestrian and cycling connections and the renaturalized Don River. New public street connections *provide permeability* into, out of and within the Port Lands. The public streets *promote synergies* between the South of Eastern area and the Port Lands by stitching these two areas together, and better connect the Port with the rest of the city. The Port Lands' *unparalleled views*, including those of the city's skyline, are protected, framed by development and celebrated. *New views* to the water's edge, river valley and iconic structures are created.

### LEVERAGING THE PORT LANDS ASSETS

The Port Lands are an important remnant of the city's *industrial past* and portions have since evolved into *wonderfully, diverse natural areas*. There are a number of important *and iconic heritage resources* that are conserved, repurposed and appropriately leveraged to contribute to *placemaking* and to *celebrate the Port Lands industrial heritage*. The new Lake Ontario Park, which includes Tommy Thompson Park, the Base Lands and Leslie Spit, is a key asset that distinguishes the Port Lands as a *unique destination* for people, and *provides habitat for wildlife*.

### DEVELOPING A HIGH-QUALITY PUBLIC REALM

A comprehensive network of *public parks and open spaces* are developed that capitalizes on the Port Lands' *waterfront setting, the new river valley, the Ship Channel and Lake Ontario Park*. High-quality streetscapes, outstanding parks, new natural linkages and design excellence for public facilities are secured to ensure that complete communities created in the Port Lands are great places to live, work and visit.

### CONTRIBUTING TO THE SUSTAINABLE FUTURE OF THE CITY

The *dynamic mix of uses* developed in a walkable, urban form, *the creation of new jobs and opportunities* and *continued port activity* are the cornerstones of the Port Lands' and city's *sustainable future*. Equally important is ensuring that all aspects of redevelopment contribute to a healthy and sustainable environment. Leading-edge and innovative approaches are utilized that *showcases* the revitalized Port Lands as a leader of sustainable development on the world's stage. Reducing resource consumption, providing low-carbon developments, minimizing dependency on the private automobile and fostering new technologies are just some of the principles that are employed to *optimize* the sustainability of the revitalized Port Lands.

### PROVIDING FLEXIBILITY AND CERTAINTY IN THE PLAN'S IMPLEMENTATION

The Port Lands, at 356 hectares, will incrementally redevelop over an *extended period of time*. The planning framework for the Port Lands must allow for a *high degree of flexibility* to accommodate changes over time. Notwithstanding this flexibility, it *must also be specific enough* to ensure that public and private investments contribute to the long-term vision for the Port Lands and have *lasting value*.

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## Sub Areas

What types of land uses or character do you think the sub areas in the Port Lands should have?

